

MICHIGAN STATE HIGHWAY DEPARTMENT  
John C. Mackie  
State Highway Commissioner

PLACE CLASSIFICATION  
FOR  
TRUNKLINE SELECTION  
(Revision of 1964)

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Prepared By  
Resource Transportation Planning Unit  
Office of Planning  
June, 1964

# OFFICE MEMORANDUM



MICHIGAN

STATE HIGHWAY DEPARTMENT

JOHN C. MACKIE, COMMISSIONER

June 10, 1964

To: E. A. Bellenbaum  
Chief Planning Engineer

From: H. G. Bauerle, Director  
Resource Transportation Planning

A handwritten signature in cursive script, appearing to read "H. G. Bauerle".

Subject: Place Classification for Trunkline Selection  
(Revision of 1964)

The attached report is a revision and updating of previous work in place classification for trunkline selection.

It is the attitude of this unit that route classification is made considerably less difficult if the place classification is well established. The necessary connecting routes can be determined by the community activity and the anticipated volumes to be accommodated.

The work contained herein was prepared by Randolph B. Lutz of this unit after consultation with members of the Planning Division.

Further refinement of this report will be possible after later census and business data become available.

HGB:ff

att

## PLACE CLASSIFICATION FOR TRUNKLINE SELECTION

(Revision of 1964)

SECTION I of Act 51, P. A. 1951, provides that: "The State Highway Commissioner may, from time to time, make and establish such subordinate classifications or groupings of state trunkline highways as he deems necessary or desirable for proper administration of the state trunkline highway system. Additions to and deletions from the state highway trunkline system may be made from time to time in the manner prescribed by law."

The transportation planning study now in progress as a part of the State Resource Development Planning Program (701) requires that a skeletal trunkline system be established which will be adequate for the anticipated movements of interstate, intercity and intraregional traffic through the target years of 1980 and 2000.

The State Highway Department has a rational method of classifying highways, based broadly upon the classification of the places which they interconnect and serve, in terms of relative economic activity. This method was developed in a study of highway function and service which was begun in 1944 and completed in 1947. A description of the basic study can be found in the report, "A Method of Rural Road Classification", presented before the Highway Research Board at their 1949 meeting.

Briefly, highway classification by this method consists of first determining the relative traffic attraction of the significant destinations of travel in the state, and then classifying the highway routes

connecting these destinations according to the determined importance of the places themselves. The basic study produced a register of 1,300 places for which the relative traffic attraction had been determined. These places were grouped into classes representing marked differences in importance as destinations of traffic; and five of these classes, containing 138 places in all, were found to have traffic attraction indicating the need for trunkline service.

In the twenty years which have elapsed since the inception of the highway classification study, the population of the state has increased by half, and the pattern of population distribution has changed significantly. Both the social and the economic patterns also are changing, more rapidly and more radically in some areas than in others. All of these phenomena affect, and continually change, the requirements for efficient highway transportation service. The most significant factors, in terms of their reflected needs in transportation, are:

The absolute increase in population, resulting in more highway travel and greater highway needs.

Population shifts, highlighted by the emergence of great urban complexes, causing changing traffic patterns and concentrations.

Changing land uses, especially the spread of commerce and industry into heretofore rural areas.

Higher personal income, stimulating a remarkable increase in motor vehicle ownership.

Shorter work week and paid vacations, resulting in greatly increased motor vehicle use.

Phenomenal increases in outdoor activities, extending throughout the state and through the four seasons of the year.

In this changing environment, it is vital that the basic place classification be re-examined in relation to the changes which have occurred in the past twenty years, in order to have sound groundwork on which to project the further changes which may have occurred by the target years of the State Resource Development Planning Program.

This has been done, using merchant wholesale trade, retail trade, bank deposits and selected services as the principal criteria. Other factors also were given consideration - particularly in cases which were borderline as judged by the principal criteria - such as:

Value added by manufactures	Airports and landing fields
Seats of government	Recreation and sports
Newspaper circulation	Culture and higher education
Medical and hospital facilities	State and Federal institutions
Lake ports and shipping	Auto and car ferry service

Unfortunately, much of the data for both the major and the supporting criteria is either incomplete or out-of-date. The major criteria are based on the U. S. Census of Business of 1958, in which wholesale statistics were compiled for cities over 5,000 population, retail trade and selected services for cities over 2,500 population, and manufacturing for cities over 10,000 population according to the 1950 Census of Population. It follows that statistics are lacking for places which attained these respective levels of population in 1960. A new Census of Business was taken in 1963, but the reports are not as yet available.

The results of this review are presented herewith as "Place Classification for Trunkline Selection, Revision of 1964", with the expectation that it will serve as a tool at least for the beginning stages of the state transportation planning study. Needed revisions can be made as new data becomes available, and as the progress of the transportation study dictates.

In this review an attempt has been made to maintain compatibility with the original study, but inevitably there have been changes in place class titles and definitions. The roster still contains five classes of places at the trunkline level of importance, but growth and change in the state have dictated changes in nomenclature and greater precision in definition. Perhaps the most noteworthy change is the decision to treat the Detroit Urban Area separately and apart from the place classification, for these reasons:

1. It is a national metropolitan center and as such should be classified at the interstate, rather than the state, economic level.
2. It must be considered as a region rather than a city, embracing other cities whose own economic importance is overshadowed by the proximity of the central city.
3. The classifying of subordinate cities and other traffic generators within the Detroit Urban Area is a study which should be undertaken of itself, and not as a part of a state classification study.

The Detroit Urban Area is regarded then as a kind of super-class, a place of highest importance in the national economy and a national hub of transportation and communication. In industry, banking and finance, wholesale trade and other significant indices, it ranks with Chicago and New York rather than with other places within the state. It is the focus of three Interstate highways; it is an international port of entry, a major Seaway port, a major airline terminal and an important rail hub. Almost half of the population of the state live and work within this urban area, and Detroit itself is the cultural, artistic and professional sports center of Michigan.

This leaves five classes of places at the trunkline level of traffic attraction, as in the original study, but with class titles and definitions revised as follows:

- I. Metropolitan Center. A place which offers complete market, service, distribution, financial, professional and health facilities for a large trade area embracing many trade areas of lesser importance, and serves as one of the major centers in the state economic structure.
- II. Regional Center. A place which offers complete market, health, service and professional facilities for its trade area, which includes several lesser trade areas, and serves as a banking and distributing center. It may be outstanding in one or more economic categories, such as manufacturing, processing, or Great Lakes shipping.
- III. District Center. A place which offers extensive market, service, health and professional facilities to its trade area, and is a minor distributing and banking center. It may be important because of a commercial airport or a Great Lakes port facility.
- IV. Area Center. A place which offers essential market and service facilities to a limited trade area, or which is a county seat. It has limited health and professional services.
- V. Special Interest Center. A place which may offer limited market and other services to a small area, but which is important chiefly as (1) a center of recreational or cultural activity, (2) the seat of an institution, educational or otherwise, or (3) a terminal area, or "farthest point" geographically.

A roster of "Classified Trunkline Places in Michigan" is appended to this report. This roster contains the place name, county, place population, approximate size of trade area, and approximate population of trade area for 13 Metropolitan Centers, 14 Regional Centers and 40 District Centers; the place name, county, and place population for 58 Area Centers and 14 Special Interest Centers. In all, the roster lists 139 places (excluding the Detroit Urban Area) at the trunkline level of economic importance.

CLASSIFIED TRUNKLINE PLACES IN MICHIGAN

Place	County	1962 Population	Approximate Trade Area		Remarks
			Sq. Mi.	Pop.	
<u>CLASS I: Metropolitan Centers</u>					
Flint	Genesee	196,940	1,680	466,000	
Grand Rapids	Kent	177,313	4,820	585,000	
Lansing	Ingham	107,807	2,360	324,000	
Saginaw	Saginaw	98,265)	8,480	563,000	Common trade area
Bay City	Bay	53,604)			
Ann Arbor- Ypsilanti	Washtenaw	88,297	880	180,000	
Kalamazoo	Kalamazoo	82,089	1,700	255,000	
Jackson	Jackson	50,720	1,540	196,000	
Marquette	Marquette	19,824	2,480	64,000	
Sault Ste. Marie	Chippewa	18,722	3,920	52,000	
Traverse City	Grand Traverse	18,432	2,070	66,000	
Escanaba	Delta	15,391	2,730	48,000	
Alpena	Alpena	14,682	2,420	52,000	
Totals		942,086	35,080	2,851,000	
Percent of State		12	62	36	

CLASS II: Regional Centers

Muskegon-Muskegon Heights	Muskegon	66,037	1,220	195,000	
Battle Creek	Calhoun	44,169	1,540	185,000	
Port Huron- Marysville	St. Clair	40,149	1,760	143,000	
Benton Harbor- St. Joseph	Berrien	30,891	680	131,000	
Holland-Zeeland	Ottawa-Allegan	28,479	360	58,000	
Menominee- Marinette	Menominee	24,618	510	20,000	Area in Michigan
Iron Mountain- Kingsford	Dickinson	15,446	1,080	26,000	Area in Michigan
Mt. Pleasant	Isabella	16,139	630	38,000	
Ironwood-Hurley	Gogebic	13,028	1,560	28,000	Area in Michigan
Cadillac	Wexford	10,112	1,260	28,000	
Hancock-Houghton	Houghton	8,415	2,350	49,000	
Iron River-Caspian- Stambaugh	Iron	8,016	1,910	20,000	Area in Michigan
Petoskey	Emmet	6,138	1,700	39,000	
Cheboygan	Cheboygan	5,859	770	15,000	
Totals		317,496	17,330	975,000	
Percent of State		4	30	12	



Place	County	1962 Population	Approximate Trade Area		Remarks
			Sq. Mi.	Pop.	
<u>CLASS III: District Centers</u>					
Midland	Midland	27,779	480	48,000	
Monroe	Monroe	22,968	500	90,000	
Adrian	Lenawee	20,347	720	76,000	
Owosso-Corunna	Shiawassee	19,770	410	45,000	
Ishpeming-Negaunee	Marquette	14,983	730	21,000	
Niles	Berrien	13,842	420	66,000	
Grand Haven	Ottawa	11,066	170	28,000	
Manistee-East Lake	Manistee	9,610	570	19,000	
Ludington	Mason	9,421	620	24,000	
Alma	Gratiot	8,978	710	43,000	
Sturgis	St. Joseph	8,915	170	17,000	
Coldwater	Branch	8,880	420	33,000	
Big Rapids	Mecosta	8,686	540	19,000	
Charlotte	Eaton	7,657	410	28,000	
Hillsdale	Hillsdale	7,629	690	38,000	
Greenville	Montcalm	7,440	580	35,000	
Three Rivers	St. Joseph	7,092	380	23,000	
Ionia	Ionia	6,754	460	31,000	
Marshall	Calhoun	6,736	230	15,000	
Hastings	Barry	6,375	360	22,000	
Lapeer	Lapeer	6,160	650	42,000	
South Haven	Van Buren	6,149	290	24,000	
Fenton	Genesee	6,142	200	24,000	
St. Johns	Clinton	5,629	290	18,000	
Manistique	Schoolcraft	4,875	1,020	9,000	
Howell	Livingston	4,861	440	32,000	
Allegan	Allegan	4,822	180	11,000	
Rogers City	Presque Isle	4,722	440	10,000	
Tawas City- East Tawas	Iosco	4,272	830	22,000	
Munising	Alger	4,228	1,100	8,000	
St. Ignace	Mackinac	3,334	630	7,000	
Bad Axe	Huron	2,998	980	37,000	
Charlevoix	Charlevoix	2,751	110	5,000	
Newberry	Luce	2,612	1,350	10,000	
Clare	Clare	2,442	610	13,000	
L'Anse	Baraga	2,397	740	7,000	
Ontonagon	Ontonagon	2,358	540	6,000	
Crystal Falls	Iron	2,203	410	4,000	
West Branch	Ogemaw	2,025	820	10,000	
Grayling	Crawford	2,015	490	4,000	
Totals		311,923	21,690	1,024,000	
Percent of State		4	38	13	

Place	County	Population	Approximate Trade Area		Remarks
			Sq. Mi.	Pop.	
<u>CLASS IV: Area Centers</u>					
Albion	Calhoun	12,749			
Dowagiac	Cass	7,208			
Tecumseh	Lenawee	7,045			
Buchanan	Berrien	5,341			
Gladstone	Delta	5,267			
Grand Ledge	Eaton	5,165			
Montague					
Whitehall	Muskegon	4,956			
St. Clair	St. Clair	4,538			
Mason	Ingham	4,522			
Marine City	St. Clair	4,404			
Calumet-Laurium	Houghton	4,197			
Otsego	Allegan	4,142			
Eaton Rapids	Eaton	4,052			
St. Louis	Gratiot	3,808			
Caro	Tuscola	3,534			
Fremont	Newaygo	3,384			
Romeo	Macomb	3,327			
Bessemer	Gogebic	3,304			
Wakefield	Gogebic	3,231			
Norway	Dickinson	3,171			
Plainwell	Allegan	3,125			
Paw Paw	Van Buren	2,970			
Boyne City	Charlevoix	2,797			
Ithaca	Gratiot	2,611			
Gaylord	Otsego	2,568			
Brighton	Livingston	2,282			
Frankfort-					
Elberta	Benzie	2,242			
Gladwin	Gladwin	2,226			
Reed City	Osceola	2,184			
Sandusky	Sanilac	2,066			
Cassopolis	Cass	2,027			
Hart	Oceana	1,990			
East Jordan	Charlevoix	1,919			
Frankenmuth	Saginaw	1,728			
Kalkaska	Kalkaska	1,321			County seat and largest place
Lake Linden	Houghton	1,314			Lake shipping port
Scottville	Mason	1,245			Garden produce center
Standish	Arenac	1,214			County seat and largest place
Houghton Lake					
Heights	Roscommon	1,195			Resort (summer pop. 3,000), largest
Mancelona	Antrim	1,141			Largest place in county
Stanton	Montcalm	1,139			County seat
Harrison	Clare	1,072			County seat and resort center
Elk Rapids	Antrim	1,015			Resort and recreation center
White Cloud	Newaygo	1,001			County seat
Centreville	St. Joseph	971			County seat
Mackinac City	Emmet-Cheboygan	934			Bridge terminal, Mackinac Island Ferry
Roscommon	Roscommon	867			County seat
Benzonia-Beulah	Benzie	843			County seat, resort center

Trunkline Places Con't.

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Place	County	Population	Remarks
Baldwin	Lake	835	County seat, largest place in county
Lake City	Missaukee	718	County seat, largest place in county
Bellaire	Antrim	689	County seat, resort center
DeTour	Chippewa	669	Ferry to Drummond Island
Mohawk	Keweenaw	650	Largest place in county
Mio	Oscoda	500	County seat; summer pop. 800
Harrisville	Alcona	487	County seat, largest place in county
Atlanta	Montmorency	450	County seat, largest place in county
Leland	Leelanau	400	County seat; summer pop. 800
Eagle River	Keweenaw	60	County seat; summer pop. 200

CLASS V: Special Interest Centers

Milan	Washtenaw-Monroe	3,616	Federal and state institutions
Durand	Shiawassee	3,312	Railroad center
Algonac	St. Clair	3,190	Access to Harsens Island
Saugatuck- Douglas	Allegan	1,529	Resort and artists colony
Newaygo	Newaygo	1,447	Services for summer residents (lakes)
Harbor Springs	Emmet	1,433	Resort and services to ski areas
Olivet	Eaton	1,185	Educational institution
Port Austin	Huron	706	Farthest point in "Thumb" area
Northport	Leelanau	700	Largest in county and farthest point
Grand Marais	Alger	600	Area service and farthest point
Cedarville	Mackinac	300	Summer pop. 2,000; access to Cheneaux Is.
Old Mission	Grand Traverse	80	Summer pop. 400; farthest point
Copper Harbor	Keweenaw	60	Summer pop. 250; farthest point on Keweenaw Pen. and boat to Isle Royale
Interlochen	Grand Traverse	50	Summer pop. 7,000; music festivals

DETROIT URBAN REGION

The following statistics on the Detroit Urban Region are included as basic information for a future economic study and classification of intraregional traffic attraction centers.

Identification	Approximate Area	Approximate Pop.
Detroit, Highland Park and Hamtramck	145 Sq. Mi.	1,742,000
Detroit Urban Region	730 Sq. Mi.	3,500,000
Immediate Trade Area	2,375 Sq. Mi.	3,800,000

Within the Detroit Urban Region are several cities which would be classified at the regional or district level if considered apart from their satellite status within the region. They are listed here and rated in economic importance as B, C, and D, corresponding generally with the statewide classifications II, III and IV.

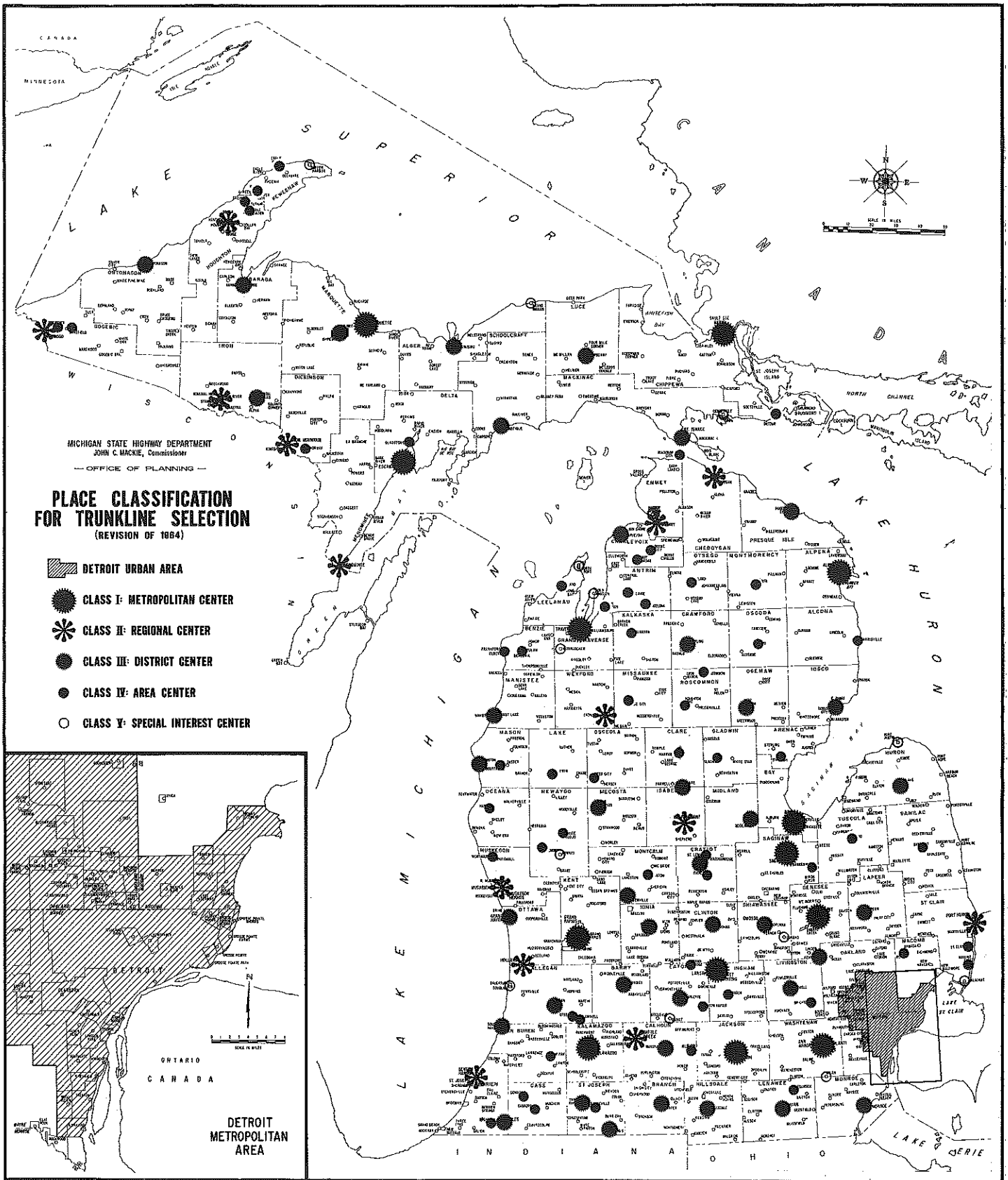
Class	City	Approx. Pop.	Reasons for Classification
B	Ferndale	31,000	Wholesale and retail trade
B	Mt. Clemens	21,000	Wholesale and retail trade
B	Pontiac	82,000	Industry; retail trade
C	Birmingham	26,000	Retail trade
C	Dearborn	112,000	Industry; retail trade
C	Wayne	16,000	Industry; retail trade
C	Wyandotte	44,000	Industry; major lake port
D	Ecorse	17,000	Industry
D	Livonia	67,000	Retail trade; industry potential
D	Northville	4,000	Retail trade; state institution
D	Plymouth	9,000	Retail trade
D	River Rouge	18,000	Industry; major lake port
D	Trenton	18,000	Major lake port

#### A P P E N D I X

Appended also to this report is a list of 15 minor named places in Michigan which have a summer population of 2,500 or more, as contrasted with a normal population ranging from 50 (Interlochen) to 3,518 (Paw Paw Lake).






MINOR NAMED PLACES WITH SUMMER POPULATION OF 2,500 OR MORE

<u>Place</u>	<u>County</u>	<u>Normal Pop.</u>	<u>Summer Pop.</u>
Manitou Beach	Lenawee	1,544	8,000
Union Pier	Berrien	900	8,000
Interlochen	Grand Traverse	50	7,000
Sister Lakes	Van Buren	100	5,000
Clarklake	Jackson	400	4,000
Lakeland	Livingston	500	4,000
Paw Paw Lake	Berrien	3,518	4,000
Whitmore Lake	Washtenaw	900	4,000
Coldwater Lake	Branch	500	3,000
Crystal	Montcalm	400	3,000
Houghton Lake Heights	Roscommon	1,195	3,000
Indian River	Cheboygan	300	3,000
Klinger Lake	St. Joseph	200	3,000
Lake Fenton	Genesee	1,500	3,000
Sawyer	Berrien	1,300	2,500



MICHIGAN STATE HIGHWAY DEPARTMENT  
 JOHN C. MACKIE, Commissioner  
 — OFFICE OF PLANNING —

**PLACE CLASSIFICATION  
 FOR TRUNKLINE SELECTION**  
 (REVISION OF 1984)

-  DETROIT URBAN AREA
-  CLASS I: METROPOLITAN CENTER
-  CLASS II: REGIONAL CENTER
-  CLASS III: DISTRICT CENTER
-  CLASS IV: AREA CENTER
-  CLASS V: SPECIAL INTEREST CENTER

